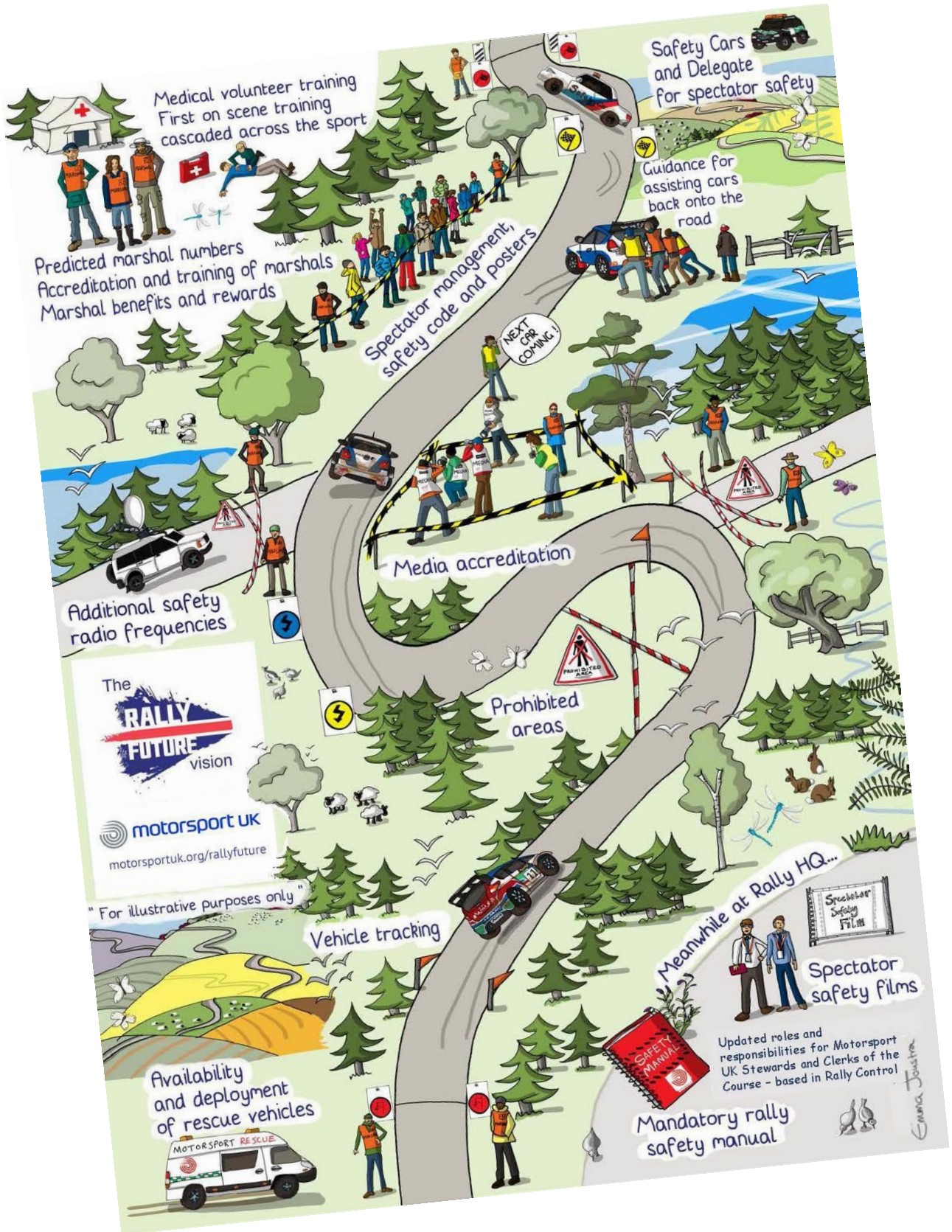


**Edition 4 Annex Summary Document Stage Rally
Safety Requirements**



The update to the Stage Rally Safety Requirements (SRSRs) Annexes is intended to update, clarify or supplement the April 2018 SRSRs Document. The additions and amendments have been collated based on input from various sources including: Rallies Committee, Rally Forums, Safety Delegates, Clerks of the Course and general input from events, Marshals and Officials. We are grateful for the time you have all taken to help update this document.

THIS SUMMARY SHEET IS NOT INTENDED TO BE USED AS A STANDALONE DOCUMENT BUT AS A SUPPLEMENT TO the SRSRs V4. A fully updated version of the respective Annexes (dated May 2019) is available on the Motorsport UK website.

Annex	Amendment/Clarification/Addition or Update
<p>Annex A – Guidance on Content of Safety Plans</p>	<p>10. The telephone numbers of the various emergency and safety services:</p> <ul style="list-style-type: none"> a. Police b. Hospitals (including address) c. Fire Service d. First Aid and casualty transport e. Ambulance Service f. Rescue Units located at each special stage g. Recovery units located at each special stage h. Doctor(s) &/or Paramedics located at each special stage i. Others as appropriate <p>NB The hospitals selected should be contacted in writing before the rally requesting that the emergency services be placed on standby.</p> <p><i>Consideration should be given to the transport of casualties to hospital under inclement weather conditions. This is particularly important when local facilities rely on helivac.</i></p>
<p>Annex B - Guidance on Content of Incident Management Plans</p>	<p>Incident Management Plans</p> <p>10. <i>When dealing with an incident where no medical intervention is required; it is worth ensuring that all officials understand the procedure that you wish to be used for the reporting of any stage blockage, whether a partial or full blockage. How should these situations be reported? Who should do what? What descriptions should the radio operators use to describe the situation? For example:</i></p> <ul style="list-style-type: none"> a. <i>Clear</i> b. <i>Passable with care</i> c. <i>Blocked</i> <p>11. <i>As any of the above situations could involve a red flag situation it is important that both officials and competitors are aware of the procedure. The radio operators notes and the competitor briefing are the ideal places to share this information.</i></p>

<p>Annex F - Spectator Set Up Diagrams</p>	<p>All Spectator Viewing Area diagrams should include the following:</p> <ul style="list-style-type: none"> • Indication of the location of PROHIBITED AREA signs (Motorsport UK Notice B). <i>Ensure the area is clearly defined.</i>
<p>Annex G - Marshal Information</p>	<p>To ensure you are covered by Motorsport UK insurance, you are required to sign-on. <i>Signing on</i> is taking place at </p> <p>REPORTING TO YOUR STAGE You should ensure that you know –</p> <ul style="list-style-type: none"> • Where the nearest red flag <i>point</i> is to your location; • <i>What to do in the event of an incident or stage blockage (partial or full)</i> <ul style="list-style-type: none"> • Report circumstances as quickly as possible to radio crew if nearby or stop the next competing car and ask them to report: <ul style="list-style-type: none"> ○ <i>Location;</i> ○ Whether blocking stage <i>and to what extent (clear, passable with care, blocked);</i> • <i>If a car either goes off or breaks down at your location, ensure you know what to do:</i> <ul style="list-style-type: none"> ○ <i>Follow the Motorsport UK Assisting Cars Code</i> <p>CLEAR</p> <ul style="list-style-type: none"> ○ <i>Are the competitors OK – have they put out their OK sign and warning triangle? To be effective, approximately 50 - 100 metres before and off the competing line but clearly visible to oncoming competitors. if so, ensure they move well away from the vehicle into a suitable location.</i> ○ <i>If the competitors are not OK – send a colleague to warn you of oncoming competitors</i> ○ <i>Put out their SOS sign and warning triangle and stop the next competitor to appraise them of the situation. They should know what to do next. Their role is to wait until the following car arrives and pass on whatever information they have gleaned.</i> ○ <i>The second car on scene should then travel to the next radio point to advise them so they can let Rally Control know what the situation is. Try to provide this second crew with as many FACTS as you can, avoid expressing your opinions and conjecture.</i> ○ <i>If it is possible to pass the incident, let the following crews continue to the end of the stage to avoid any blockage.</i> <p><i>Await the rescue unit and medical teams. If you have received First Aid or First on Scene training and feel comfortable doing so, please put it to good use.</i></p>

Annex I – Stage Rally Safety Car Guidelines

General Information – all Safety Cars

As each event attended is different in the way the stages are presented with the safety plan and the documentation is prepared by different event teams. Always ensure you have received all your paperwork in sufficient time to prepare fully. As part of your preparation you should consider the following:

Pre-event preparation for example –

<i>Clip Board</i>	<i>Pens/Pencils (including permanent marker)</i>
<i>Staple Gun and Staples</i>	<i>Video camera, charger and fixing mount (ensure it does not obstruct the airbag)</i>
<i>Maps</i>	<i>Event Paperwork</i>
<i>Personal preparation including food and drink</i>	<i>Suitable clothing</i>
<i>Motorsport UK marshal's tabard & whistle</i>	<i>Motorsport UK Safety Car Licence</i>
<i>Personal First Aid Kit</i>	<i>Mobile phone and charger</i>
<i>PA/Sirens/Lights</i>	<i>Radio – fitted and working</i>

Obviously, there are many more items to prepare, refer to your notes from the Safety Car workshop for further information.

When installing dash cams, tablets, trip meters, mobile phones etc, please ensure they are not covering the airbag or obstructing the vision of the driver.

The role of the safety car is for safety purposes, they are not intended to undertake any towing and specifically, no multiple towing should be undertaken by anyone on the event.

2.12 Safety Car Team

2.12.7 The one-day Safety Car Workshop is mandatory and potential delegates will be nominated by their Clerks of Course to attend one of the Motorsport UK Programmes held each year.

2.12.8 A subsidised Driver Skills programme is available (at a personal cost) for those interested in attending. This programme is specifically designed for Safety Car Drivers however it is NOT mandatory.

~~Parts one and two of the Safety Car Workshop have now been combined, elements of part two will be incorporated into part one. This single day will be the mandatory day. Further Driver Awareness days may be available in the future.~~

SAFETY CARS AND THEIR DUTIES

11.1 For all Single-Venue stage rallies

11.1.7 All Safety Cars (excluding the zero car) should be equipped to provide information to spectators as they travel through the stage.

11.10 Interim Safety Cars

11.10.9 All Safety Cars (excluding the zero car) should be equipped to provide information to spectators as they travel through the stage.

Annex I – Stage Rally Safety Car Guidelines

Cameras for Safety Cars

Many Safety Cars are already using dashcams and this has proven significantly beneficial for reviewing post event footage. Therefore, it is a requirement that all Safety Cars, inclusive from the Spectator Safety Car to the O Car, are equipped with, and use, dashcams.

Camera Specification

There are many dashcams on the market today and it is not necessary to spend a lot of money to buy a suitable camera. From Motorsport UK Safety Delegate experiences over the last 3 years the following criteria should help select a suitable camera:

- **Resolution:** 1080p @ 30fps
- **Lens:** 130° to 180° are typical and ideal angles of view
- **Storage:** 32GB capacity is ideal. Less capacity requires more rigorous management on the event.

There are many other features such as GPS logging and Google Map recording, and a backup battery to continue recording if 12v supply is lost is strongly recommended.

Camera Installation and Use

The natural location is the co-driver windscreen (top near to the mirror) to give as uninterrupted view of the Stage as possible. It is accepted that when using a Rally-prepared car as the O Car, it may be necessary to mount the camera to the roll cage, but in so doing provide as uninterrupted view as possible. For the other Safety Cars when choosing a suitable installation take note of any airbags that may be fitted. Dashcams are to enhance safety!

It is not necessary to use voice recording facilities and we are aware that some crews may feel self-conscious and not be as focused on their duties. After the event all Stage footage should be up/downloaded or retained on the memory cards for a period of not less than 14 days. If, after that period, no queries have been identified the memory cards may be re-used.

*Even where there are no accidents or incidents, dashcam footage will allow you to review the Stages after the event, looking at setup consistency and compliance, spectator hot spots and so on. This footage **MUST** be made available to the event organisers at the earliest possible time (at most within 7 days).*

<p>Annex K - Competitor Briefing</p>	<p><i>In order to ensure that any incident is responded to by all parties in the most effective way. It is strongly recommended that ALL competitors on ALL types of stage rally are reminded of their responsibilities related to SOS/OK boards either as the crew affected by an incident or the next competing car on the scene.</i></p> <p><i>You may also wish to advise them of the Clerk of the Course option to issue discretionary times so they are aware they will not suffer as a result of stopping to assist one of their colleagues.</i></p>
<p>Annex L - Splits & Merges</p>	<p>Splits and Merges on Stage Rally events</p> <p>Competitors</p> <p>10. <i>If visibility, particularly at merges is compromised, consideration must be given to increasing time gaps between competitors.</i></p>
<p>Annex N - Notices and Posters</p>	<p>Tape</p> <p><i>There are various grades of tape, consider choosing the most appropriate for the job it's intended for. For example 30/40 micron tape may be ideal for taping gates shut or being stretched across a closed gate. It may not be ideal for box junctions or longer runs though as it has a tendency to stretch and "flap" in the wind. It is also very difficult to reuse (you can get biodegradable versions though). Tape of 50/80 microns thick is generally stronger, more suitable for spectator areas, box junctions and longer runs and does offer the chance to be reused. Bio-degradable is often available for a similar price.</i></p>